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SUBJECT: Special rept: on 920429 & 30, fuel oil leak developed on cylinder 2 & 5 right fuel injector pump delivery valve holder, respectively. Caused by through-wall cracks. All delivery valve holders replaced.

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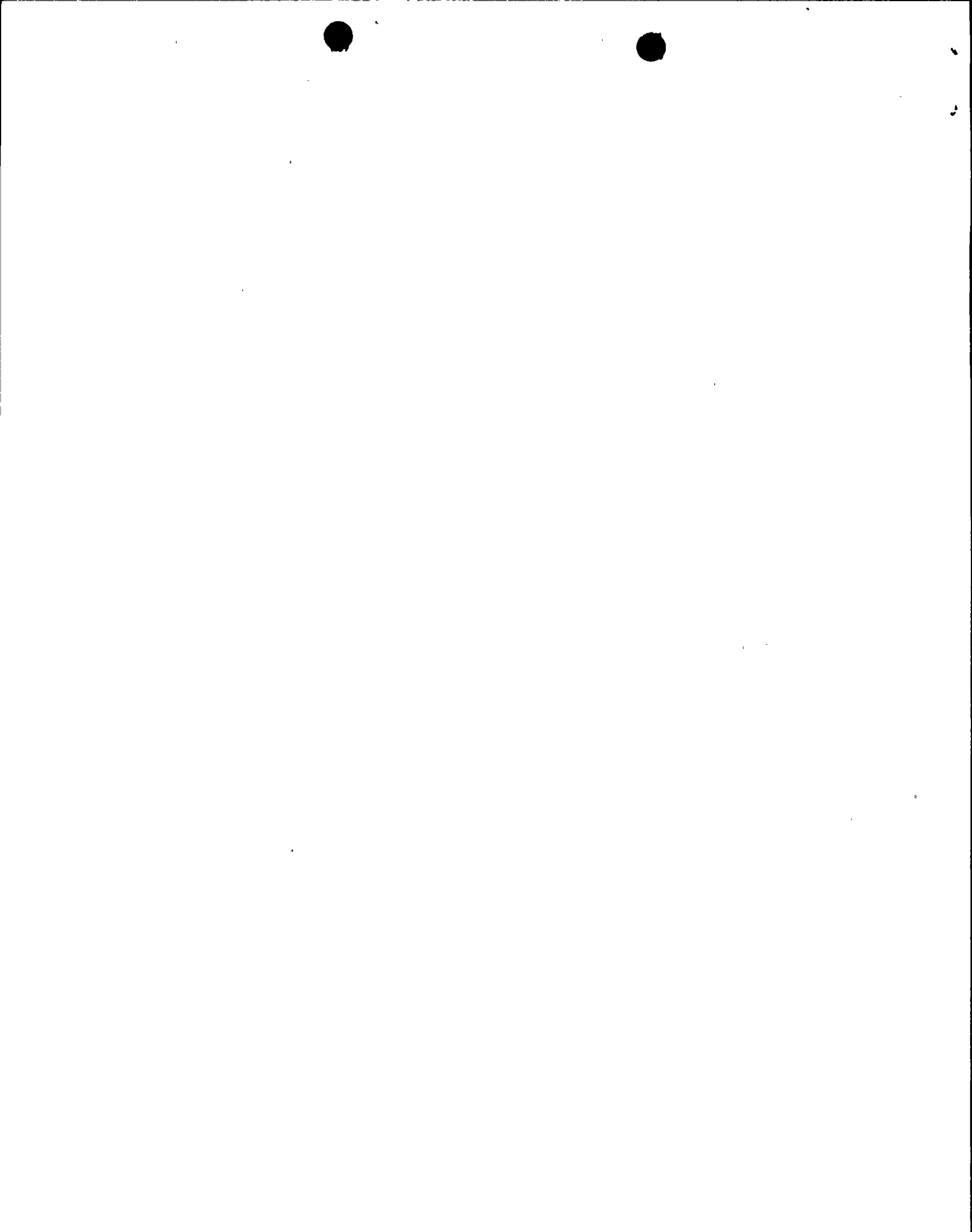
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Joseph F. Firlit
Vice President
Nuclear GenerationMay 29, 1992
NMP84884United States Nuclear Regulatory Commission
Document Control Desk
Washington, DC 20555RE: Docket No. 50-410
SPECIAL REPORT

Gentlemen:

In accordance with Nine Mile Point Unit 2 (NMP2) Technical Specification 4.8.1.1.3, we are submitting the following Special Report concerning two (2) non-valid failures (April 29, 1992 and April 30, 1992) of the Division I Standby Emergency Diesel Generator (2EGS*EG1).

SURVEILLANCE REQUIREMENTS

Surveillance testing for the Division I Emergency Diesel Generator is performed on a monthly schedule. The monthly testing interval is in conformance with NMP2 Technical Specification Table 4.8.1.1.2-1, Diesel Generator Test Schedule. The Division I Diesel Generator has experienced 0 valid failures in the last 20 valid tests and 3 valid failures in the last 100 valid tests in accordance with the test criteria set forth in Regulatory Guide 1.108.

EVENT DESCRIPTION

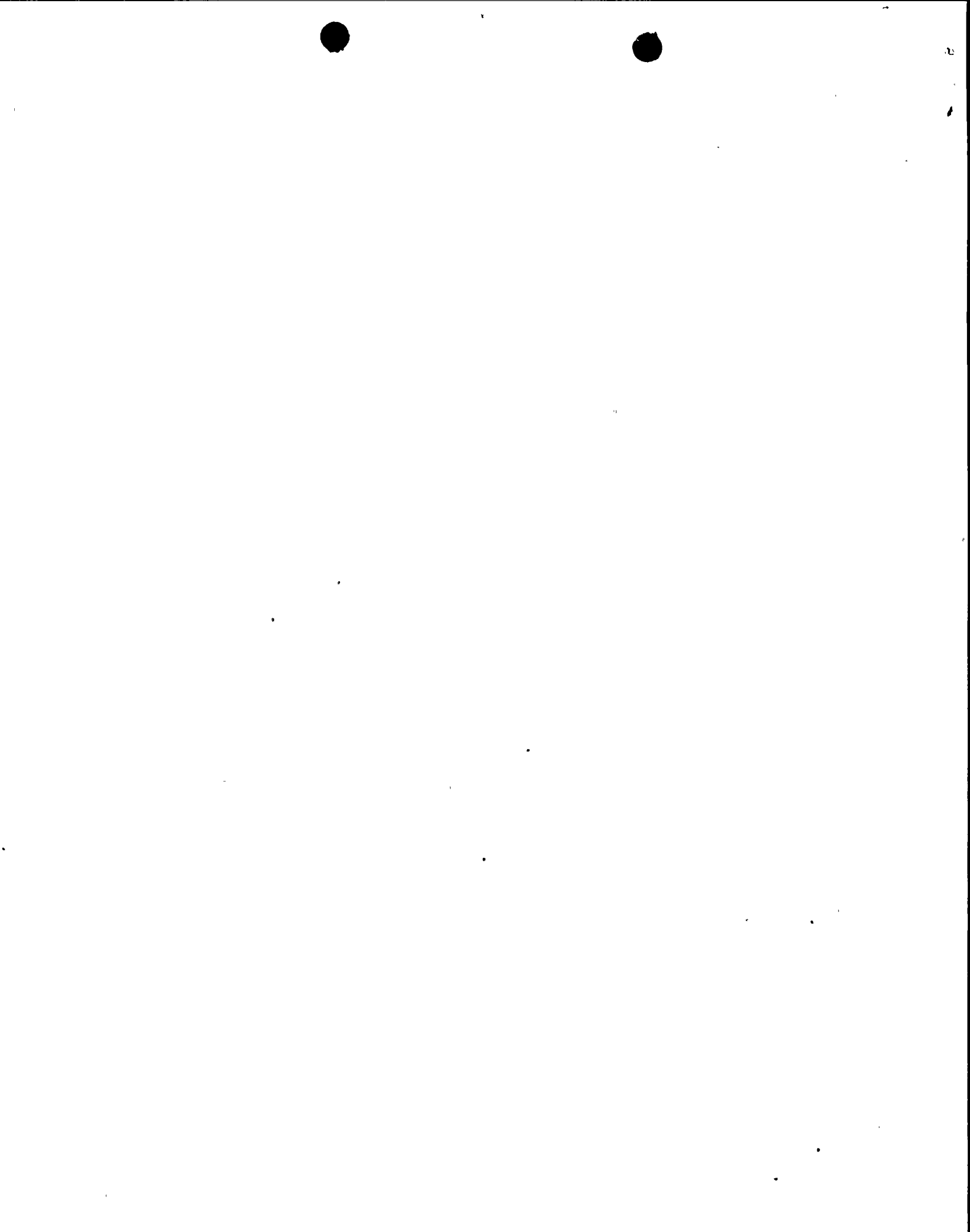
On April 29, 1992 at 1430 hours, during the performance of Operations Surveillance Procedure N2-OSP-EGS-R002, "Operating Cycle Diesel Generator Twenty-Four Hour Run and Load Rejection Divisions I and II," (at approximately 8 hours of the 24 hour run), a fuel oil leak developed on cylinder #2-Right fuel injector pump delivery valve holder. The surveillance test was terminated and the Diesel Generator was voluntarily shutdown. Prior to the 2EGS*EG1 being secured, it was producing 4400 Kilowatts (KW) and functioning as designed. A Work Request (WR #203889) was initiated to troubleshoot the cause for the leak.

On April 30, 1992 at 1020 hours, while continuing with the performance of procedure N2-OSP-EGS-R002, (at approximately 4 hours into the 24 hour run), the Division I Diesel Generator developed a second fuel oil leak. This leak was on cylinder #5-Right fuel injector pump delivery valve holder. The surveillance test was again terminated and the Diesel Generator was voluntarily shutdown. Prior to the 2EGS*EG1 being secured, it was producing 4400 Kilowatts (KW) and functioning as designed. An additional Work Request (WR #203890) was initiated to troubleshoot the cause for this leak.

ROOT CAUSE

The cause of the fuel oil leaks in cylinders #2 and #5 have been determined to be through-wall cracks in the fuel injector pump delivery valve holders. This is a previously identified failure mode which has been addressed in other Special Reports at NMP2.

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ACTIONS TAKEN

1. Deviation/Event Reports (DER #2-92-Q-1914 and DER #2-92-1994), were initiated to investigate the April 29, 1992 and April 30, 1992 Division I Diesel Generator fuel oil leaks.
2. WR #203889 was initiated and performed subsequent to the April 29, 1992 fuel oil leak to replace the #2-Right fuel injector pump delivery valve holder. Upon completion of repairs, 2EGS*EG1 was restarted and run to verify the integrity of newly installed equipment.
3. WR #203891 was initiated and performed subsequent to the April 30, 1992 fuel oil leak to replace the #5-Right fuel injector pump delivery valve holder as well as replacing all delivery valve holders which had not been recently replaced.

NOTE: 14 of 16 valve holders were replaced per this WR, 1 was replaced per WR #203889, and 1 was replaced during March 1992.

Replacement of all the delivery valve holders was made to preclude additional failures of this type. Upon completion of repairs, N2-OSP-EGS-R002 was restarted and completed satisfactorily.

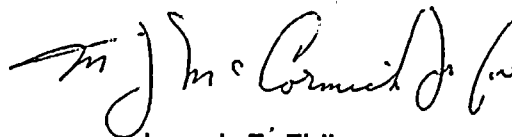
Niagara Mohawk Power Corporation is a member of Cooper-Bessemer Diesel Generator Users Group. Through participation in this group, we received Service Bulletin #748, which addresses upgrade of the high pressure fuel line assemblies on our diesels. A Plant Change Request (PCR), #PC2-0441-91, has been initiated to complete this upgrade. However, the parts described in the bulletin are not available at this time and therefore we are unable to complete modifications during our present refueling outage. Materials are on order and implementation of the PCR will be completed in accordance with plant schedules.

VALID TEST/FAILURE DETERMINATION

Investigation into the terminations of the Division I Diesel Generator surveillance test (N2-OSP-EGS-R002) has determined these terminations to be considered non-valid failures in accordance with Technical Specification Section 4.8.1.1.3 and Regulatory Guide 1.108, Section 3.b.

The Division I Emergency Diesel Generator was inoperable for scheduled routine maintenance prior to the performance of the surveillance testing.

Very truly yours,



Joseph F. Firlit
Vice President - Nuclear Generation

JFF/AZP/lmc

xc: Thomas T. Martin, Regional Administrator Region I
Wayne L. Schmidt, Senior Resident Inspector



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