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SUBJECT: Forwards response to request for addl info re application for amend for plant, Unit 2 Tech Specs dtd 890726.

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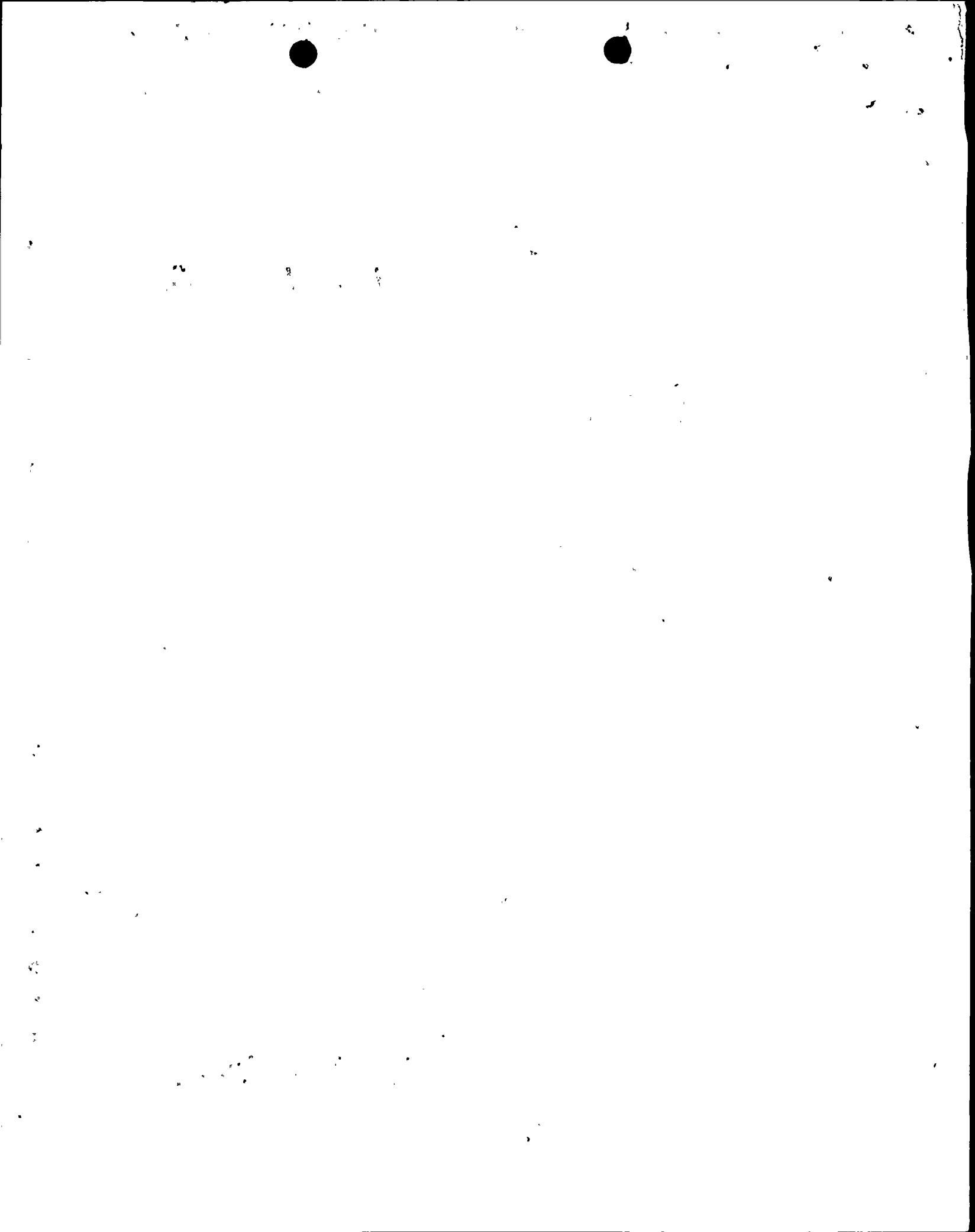
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NMP2L 1221

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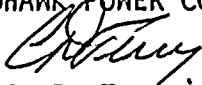
Re: Nine Mile Point Unit 2
Docket No. 50-410
NPF-69
TAC No. 74347

Gentlemen:

Niagara Mohawk submitted an Application for Amendment for the Nine Mile Point Unit 2 Technical Specifications dated July 26, 1989 (NMP2L 1203) regarding the Crane Travel-Spent Fuel Storage Pool. In a letter dated November 1, 1989, the staff requested additional information relative to the Application for Amendment. This letter provides the response to the staff's Request for Additional Information.

Very truly yours,

NIAGARA MOHAWK POWER CORPORATION


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RESPONSE TO REQUEST FOR ADDITIONAL INFORMATION

(TAC NO. 74347)

1. Please specify the specific activities desired to be carried out by Niagara Mohawk Power Corporation which result in the need for the amendment request.

Response

The spent fuel pool gates are normally installed in the fuel transfer canal immediately adjacent to the spent fuel pool. During refueling activities, the gates are relocated to their storage position on the side of the spent fuel pool. Movement of the gates to their storage location requires transfer of the gates along designated safe load paths over a portion of the spent fuel pool. The safe load path is shown on Figure 5-2 in Appendix 9C of the Nine Mile Point Unit 2 Updated Safety Analysis Report (USAR). The interlocks on the polar crane prohibit travel over the entire fuel pool and therefore they must be bypassed to allow access to the designated safe load path.

The current Technical Specifications, specifically LCO 3.9.7, prohibit loads in excess of 1000 pounds from traveling over fuel assemblies in the spent fuel pool. The LCO requires revision to allow operation in a previously evaluated mode.

2. Please identify the previously existing accident analyses relative to this issue.

Response

Movement of heavy loads and the possibility of an accidental load drop are addressed and evaluated in Appendix 9C of the Nine Mile Point Unit 2 USAR, "The Control of Heavy Loads at Nine Mile Point Unit 2". Section 3 of this appendix discusses compliance with Sections 5.1.1 and 5.1.6 of NUREG 0612, "Control of Heavy Loads at Nuclear Power Plants". Compliance with these sections provides assurance that the potential for a load drop is negligible, as stated in NUREG 0612. As discussed in the Amendment Application dated July 26, 1989, and documented in Appendix 9C of the Nine Mile Point Unit 2 USAR, Nine Mile Point Unit 2 meets the guidelines of NUREG 0612. The transfer of the spent fuel pool gates is discussed in Appendix 9C and the safe load paths are illustrated in Figure 5-2.

3. Please discuss specifically how the proposed new activities have been determined to meet each of the three criteria of the no significant hazards consideration determination.

Response

Since movement of the fuel pool gates is already fully evaluated in Appendix 9C of the USAR, the proposed change will not increase the probability of a previously evaluated accident nor will it create the possibility of a previously unevaluated accident. The change is required to permit operation in a previously evaluated mode. There is no significant reduction in any margin of safety since the existing analysis demonstrates that the probability of a load drop is negligible.

