

Assessment of Effects: Because the viewshed has been compromised by the existing transmission lines adjacent to the property, and the view of the project will be from the rear of the property, in TRC's opinion there will be no adverse effect to 51160 from the proposed project (Figure 58).

51182 (HR-10)

Within Visual APE?: Yes	NRHP Status: Ineligible	Adverse Effect: No
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This resource was previously identified by Robert Ciucevich in 1995. No modifications have occurred to the structure since that time; however, the structure has been abandoned and has fallen prey to neglect. The resource is located at the corner of Gays Circle and Wire Road, north of the proposed transmission line (see Figure 1, Sheet 1). Built in ca. 1920, the one-story wood frame hall-parlor has a ca. 1930 shed addition on the rear (west elevation). Set on a brick pier foundation, the dwelling is clad in weatherboard and the side gable roof is standing seam metal. There is an external brick chimney on the east elevation. Windows have been removed from the structure. Entry can be gained through a single leaf entry off the full width recessed porch, which is supported by wood posts (Figures 59 and 60).

Outbuilding: Constructed in ca. 1930, this one-and-a-half story, wood frame, front gable structure is covered in flush board with a standing seam metal roof. Entry is gained through an opening in the gable end, where there is also a hay loft opening, though all doors and windows have been removed. The building rests on stone piers. Shed bays which have collapsed may have once been attached to the structure (Figure 61).

NRHP Assessment: The neglect and subsequent deterioration of this structure have impacted 51182's integrity of materials, workmanship, feeling, and design. The resource retains integrity of location, association, and setting, however. But the building exhibits unremarkable design and use of materials. In accordance with 36 CFR 60.4, 51182 is recommended ineligible for the NRHP. This resource is not associated with an event or series of events, or a person(s) significant on the national, state, or local level, and is not eligible for the NRHP under Criteria A or B. The resource does not embody the distinctive characteristics of a style, period, or method of construction, nor is it the work of an architect or master builder and is not eligible for the NRHP under Criterion C. Although the transmission line will be nominally visible from this resource, intervening vegetation largely blocks the view, and an existing transmission line has already compromised the viewshed (Figure 62). Based on the NRHP assessment, TRC recommends no further work in advance of the proposed undertaking.

Newly Recoded Architectural Resources

In addition to the previously recorded historic resources, TRC identified 30 newly recorded historic structures (see Table 23). Of these resources, 19 fall within visual line of sight to the project area, while the remaining 11 are within the 0.5-km radius, but will have no view to the proposed transmission line. Six of the resources within the visual APE are recommended potentially eligible for inclusion on the NRHP. Of these, one (HR-22) will be adversely affected by the proposed undertaking. The remaining the potentially eligible resources will not be adversely affected by the project. One resource, HR-31, was not accessible due to property owner restrictions.



Figure 59. 51182, view south-southeast.

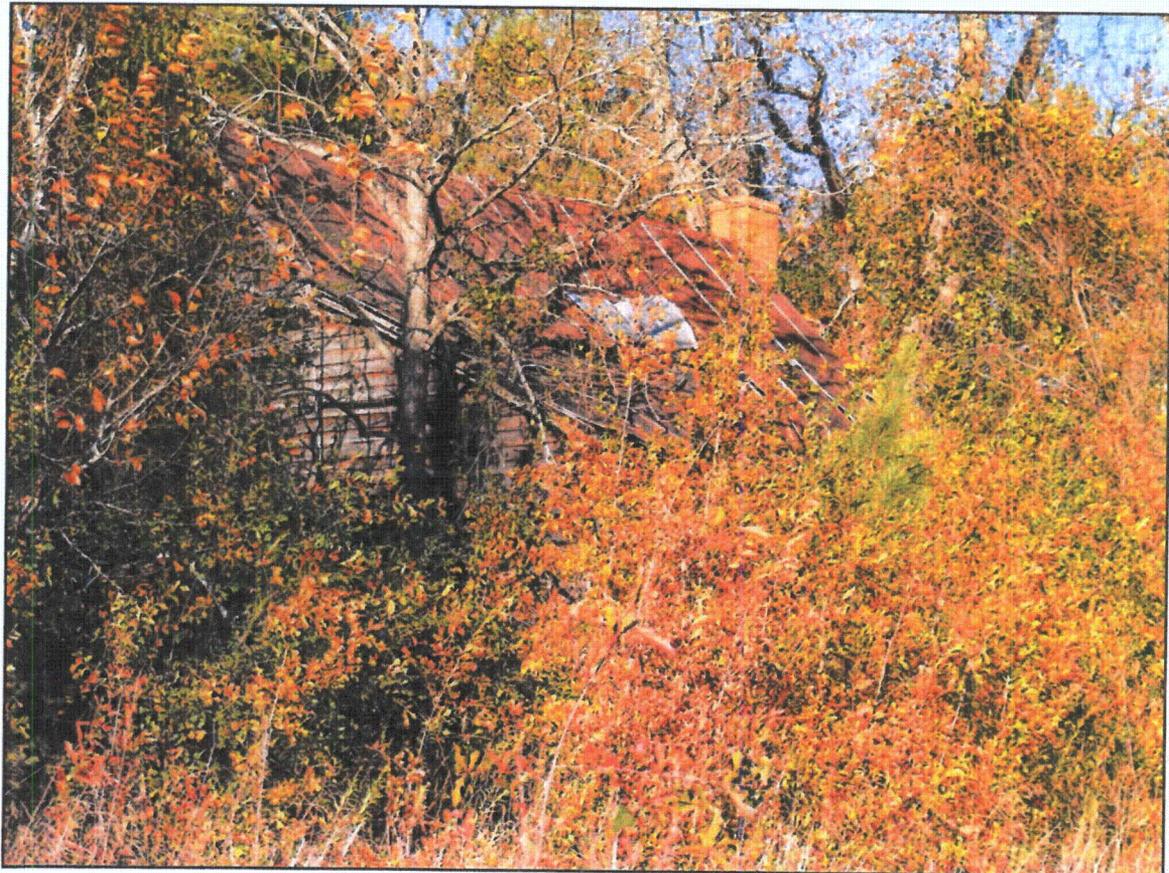


Figure 60. 51182, view northeast.



Figure 61. 51182, outbuilding, view northwest.



Figure 62. 51182 view to project area, south.

HR-2 Georgia Railroad

Within Visual APE?: Yes	NRHP Status: Potentially Eligible	Adverse Effect: No
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HR-2, the Georgia Railroad (see Figure 1, Sheet 1), was one of the first two successful lines chartered in Georgia to serve the cotton-rich hinterlands of the Piedmont. It was chartered in 1833 to build a railroad from Augusta to Athens, and construction began in 1835. The Georgia Railroad Company became the Georgia Railroad and Banking Company in 1836 and, along with the Central of Georgia Railroad and Banking Company that was building a line from Savannah to Macon, became one of the most powerful entities in the state. Augusta native John Pendleton King was president of the railroad from 1841 to 1878, and the success of Augusta was closely tied to the success of the Georgia Railroad (Boney 1977:157–158, Storey n.d.).

John Edgar Thomson was the chief engineer of the company beginning in 1834, and the town of Thomson in McDuffie County is named for him. The towns of Boneville and Dearing in McDuffie County developed around whistle stops on the railroad. The line was completed to Athens in 1841 and was extended to Atlanta in 1845. In Atlanta, the line connected with the Western & Atlantic, which opened a continuous line from Memphis to Charleston. The Georgia Railroad and Banking Company also invested in the Atlanta & West Point Railroad and the Western Railway of Alabama, which together connected their Atlanta endpoint with Montgomery (Storey n.d.).

In 1881, the Georgia Railroad was acquired by its former rival, the Central of Georgia Railway and was operated jointly by the Central of Georgia and the Louisville & Nashville lines. In the early twentieth century, the line became part of the Atlantic Coast Line, which eventually became part of the Seaboard Coast Line, the Seaboard System, and finally CSX Transportation (Storey n.d.).

NRHP Assessment: The Georgia Railroad line was evaluated for eligibility for listing in the National Register using all of the National Register Criteria for Evaluation outlined in 36 CFR Part 60.4. In the context of Criterion B, there are no known associations with individuals whose specific contributions to history can be identified and documented with this property. With regard to Criterion D, there are no indications that the property is likely to yield information on important research questions in history or prehistory. The type and method of construction of the property are not unique or unusual, and the property represents a well-documented type. The Georgia Railroad line also was evaluated under Criteria A and C and appears to possess state, regional, and local levels of significance in the areas of transportation, industrial and economic development, and engineering. In accordance with a February 17, 1995, SHPO policy paper on historic railroads in Georgia, all rail corridors in the state 50 years of age or older that retain their integrity are considered eligible for the National Register. The policy paper states:

Where historic railroads including their rail beds and any extant associated buildings and structures retain their integrity of location, design, materials, and setting, they meet National Register Criterion C as significant works of 19th or early 20th century civil engineering in Georgia. In National Register terms, they “embody the distinctive characteristics of a type, period, or method of construction.” Indeed, at the local and state level, railroads represent the single greatest engineering and construction achievement of this period. Again in National Register terms, these railroads and their associated buildings and structures, whether precedent-setting or of standardized design, collectively form “a significant and distinguishable entity.”

Georgia's historic railroads also meet National Register Criterion A for the significant role they played in the state's transportation and economic history. From trunk lines to short lines, these railroads clearly made a "significant contribution to the broad patterns of our history" at the state, regional, and local levels. Georgia's historic railroads may also be significant as archaeological resources. Archaeological investigation of historic railroads may provide information about aspects of Georgia's railroad history that are not fully understood today, including the methods and materials used to construct earthen rail beds, the location and identity of former auxiliary buildings and structures along rail lines, and the location and function of former support facilities such as maintenance and repair yards. In some instances, archaeological investigation may be required to confirm the existence of long-abandoned railroads. Where archaeological research can be expected to yield important historical information about Georgia's historic railroads, the railroad may meet National Register Criterion D.

The resource is considered eligible under National Register Criterion A for its role in the development of Georgia's transportation infrastructure and its industrial/economic base. It is considered eligible under National Register Criterion C as an example of nineteenth-century civil engineering in the state.

The Georgia Railroad has been determined to possess integrity in the areas of location, setting, design, materials, workmanship, feeling, and association. The rail line retains its historic alignment, and much of its setting from Augusta to Athens is consistent with its historic appearance. The line retains its historic design—a raised earthen and gravel bed with a single set of tracks—and although the materials have been updated over time, they are consistent with the historic materials used on the line. The present design and materials continue to illustrate the historic workmanship utilized to construct the line. Finally, with retention of the historic location, setting, design, materials, and workmanship, the feeling and associations of the rail line have remained intact.

Assessment of Effects: The proposed National Register boundary of the Georgia Railroad corresponds to the rail corridor right-of-way limits (a 150-foot width). The boundary extends the full length of the line from Athens to Atlanta as completed by 1845, but only a segment approximately 1 km in length will be subject to visual effects from the proposed undertaking (Figure 63). The proposed NRHP boundary contains all National Register qualifying characteristics and features of the property and includes the bed, ties, rails, switch elements, and immediate surrounds within the right-of-way limits. Although the viewshed of the Georgia Railroad will be affected by construction of the proposed transmission line, those visual effects are not considered adverse due to previous changes and modern intrusions to the setting along the railroad, notably the presence of existing transmission line structures.

HR-4

Within Visual APE?: Yes	NRHP Status: Ineligible	Adverse Effect: No
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Located at 3359 U.S. Highway 78, north of the proposed transmission line, this one-story concrete block commercial building was built in 1959 (MCTA 2011) (see Figure 1, Sheet 1). The structure has a flat parapet roof with a canopy attached to the façade supported by metal posts under which the gas pumps once stood (Figure 64). The rear (south elevation) of the structure



Figure 63. HR-2, Georgia Railroad, view southwest.



Figure 64. HR4, view southeast.

features two ca. 1960 additions, both of which are constructed of concrete block. An external brick chimney is present on the south elevation off the smaller addition (Figure 65). Fenestration on the façade consists of large, barred picture windows flanking a barred double leaf entry door. Entry to the main block can also be gained through a single leaf door with an aluminum awning on the east elevation. The larger addition contains four-over-four double hung wood sash windows and a sliding window on the east elevation. The smaller addition, likely containing the bathrooms, on the east side of the south elevation, has a single leaf door on the south elevation, and a single leaf door on the east elevation.

Outbuilding 1: Constructed in ca. 1960, this one-story front gable concrete block utility building has a single leaf entry door in the gable end. A six-paned fixed window is present on the south elevation. The roof is composed of compositional asphalt shingles and features exposed rafter tails, and novelty siding in the gable end (Figure 66).

NRHP Assessment: Although few modifications have occurred to the structure, and those that are present, are historic in nature, the building is a common example of its type. Furthermore, the removal of the gas pumps has affected the integrity of materials and feeling. Therefore, in accordance with 36 CFR 60.4, the HR-4 structure and outbuilding are recommended ineligible for the NRHP. The resource is not associated with an event or series of events, or a person(s) significant on the national, state, or local level and is not eligible for the NRHP under Criteria A or B. The resource does not embody the distinctive characteristics of a style, period, or method of construction, nor is it the work of an architect or master builder and is not eligible for the NRHP under Criterion C. Based on this assessment, no further work is recommended for this resource. The proposed transmission line will be visible from this resource, although intervening modern construction and an existing transmission line have already occupied the viewshed (Figure 67). Based on the NRHP assessment, no further work is recommended for HR-4 in advance of the proposed undertaking.

HR-5

Within Visual APE?: Yes	NRHP Status: Ineligible	Adverse Effect: No
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Set on a concrete block foundation, this one-story, Minimal Traditional is located at 3297 U.S. Highway 78, north of the project area (see Figure 1, Sheet 1). Tax records indicate that the dwelling was constructed in 1952 (MCTA 2011). The structure is clad in vinyl siding and has a compositional asphalt roof with a slightly off-peak brick chimney on the west elevation. Fenestration consists of paired and single six-over-six double hung replacement vinyl windows with decorative shutters. Access is gained through a single leaf door off the partial width gable porch on the façade. A secondary entrance is through a single leaf entry of the west elevation off a partial width porch. It has a gable roof supported by columns on a concrete base with a brick foundation and steps. There is a shed addition on the south elevation; materials are consistent with the main block (Figure 68 and 69). There are two modern carports and a shed on the property.

NRHP Assessment: HR-5 has retained its integrity of location, design, workmanship, feeling, and association, with few modifications, including replacement windows and wall covering, as well as a small rear addition that have compromised integrity of materials. Despite the resource's



Figure 65. HR-4, view north-northwest.

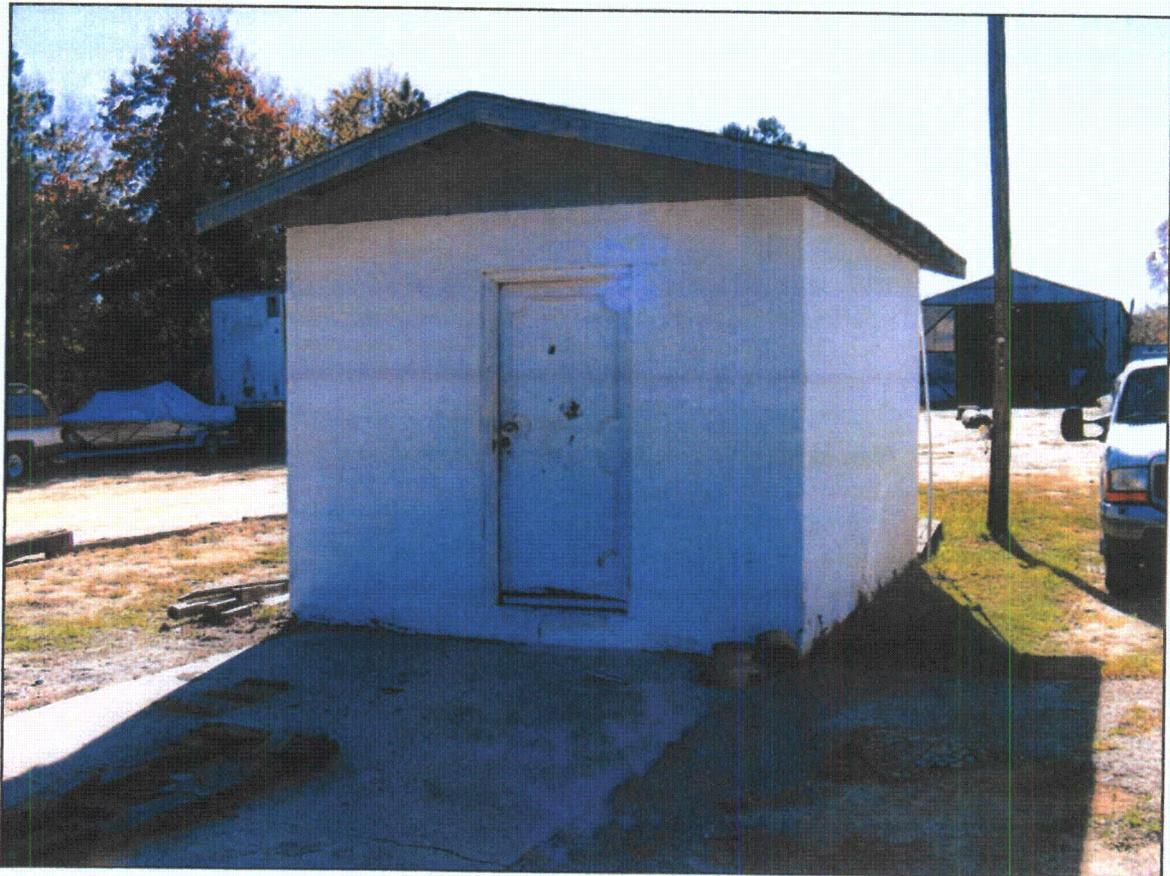


Figure 66. HR-4, outbuilding, view southeast.

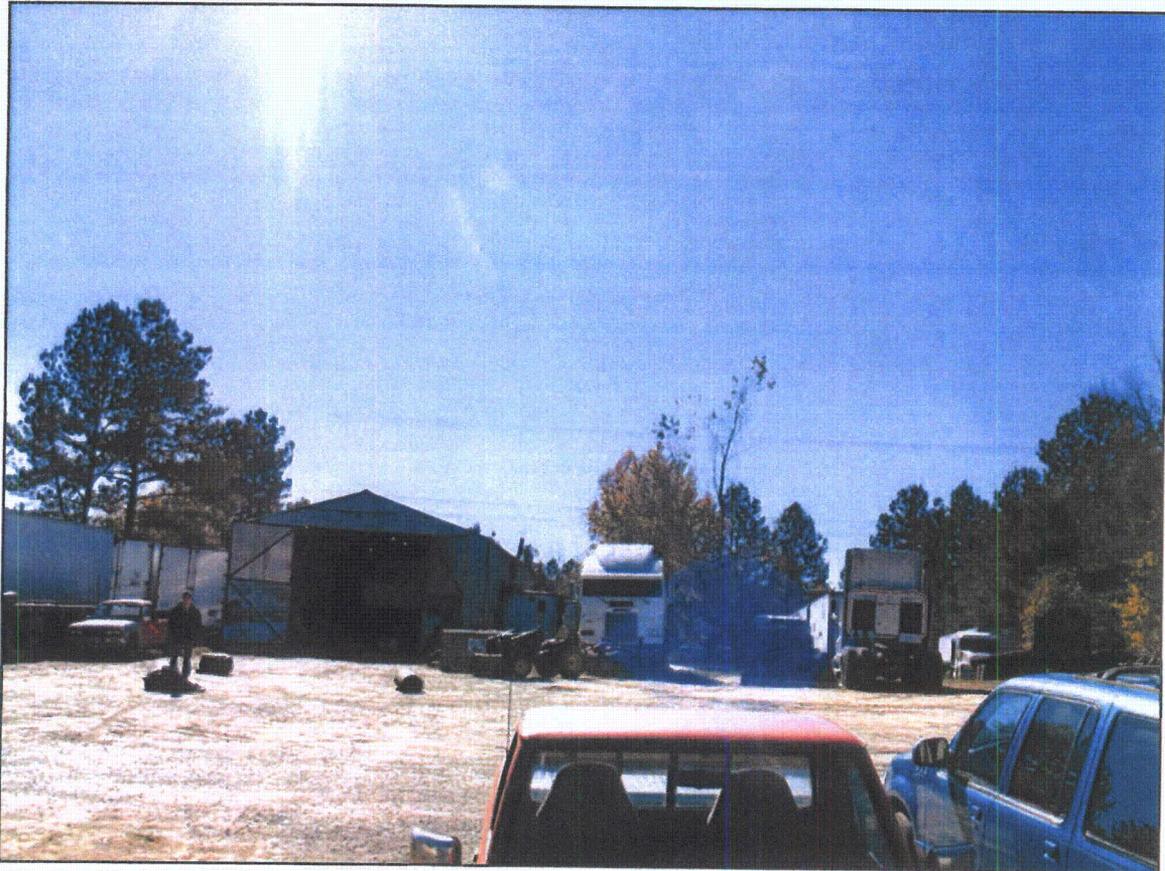


Figure 67. HR-4, view to project area, south.



Figure 68. HR-5, view southeast.



Figure 69. HR-5, view northwest.

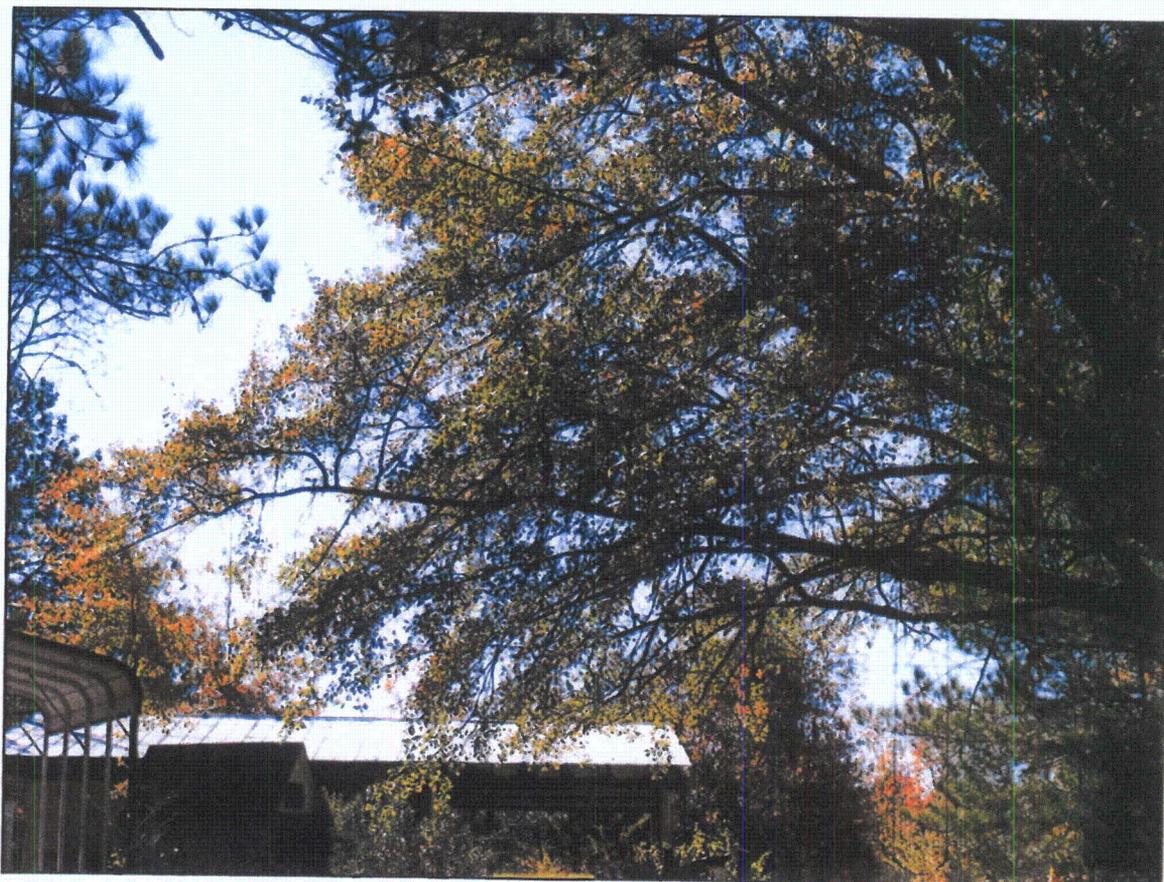


Figure 70. HR-5 view to project area, south.

overall integrity, HR-5 is a common example of its type and is therefore recommended ineligible for consideration for NRHP Criterion C. Background research undertaken by TRC has indicated that the property does not qualify for the NRHP under Criteria A or B. The research identified no known associations with significant historical events or persons. The proposed transmission line will be visible from this resource through intervening vegetation, as is an existing transmission line (Figure 70). Based on the NRHP assessment, no further consideration of this resource is recommended in advance of the proposed undertaking.

HR-8

Within Visual APE?: Yes	NRHP Status: Ineligible	Adverse Effect: No
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Set on a continuous concrete block foundation, this one-story, side gable Minimal Traditional is located at 2634 Ellington Airline Drive, south of the proposed transmission line (see Figure 1, Sheet 1). Built in ca. 1950, the structure is clad in aluminum siding and the roof is composed of compositional asphalt shingles. There is a stuccoed on-peak chimney. Fenestration consists of three-over-one double-hung wood sash windows with aluminum awnings. Entry is gained through a single leaf door of the partial-width front gable porch supported by replacement filigree metal posts resting on a concrete deck. The door surround is accented with rock-faced brick veneer. The southeast porch has been enclosed and features one-over-one double hung aluminum sash windows. A carport has been attached to the secondary porch (Figures 71 and 72). A non-historic utility building is present on the property, as is a well-house.

Well House: This ca. 1940 one-story front gable concrete block building has a roof composed of compositional asphalt shingles. There is a single leaf entry in the gable end. The cornice creates a stylized pediment in the gable end (Figure 73).

NRHP Assessment: Modifications to the dwelling including enclosure of the secondary porch, modifications to the main porch, and replacement materials such as siding, awnings, carport, and chimney have compromised the integrity of design, materials, feeling, and workmanship, so that HR-8 no longer represents its original form, making it a poor example of its type. Furthermore, the outbuilding is a common example of its type. In accordance with 36 CFR 60.4, the HR-8 dwelling and outbuilding are recommended ineligible for the NRHP. The resource is not associated with an event or series of events, or a person(s) significant on the national, state, or local level and is not eligible for the NRHP under Criteria A or B. The resource does not embody the distinctive characteristics of a style, period, or method of construction, nor is it the work of an architect or master builder and is not eligible for the NRHP under Criterion C. Based on this assessment, no further consideration is recommended for this resource. Although the transmission line will be visible from this resource (Figure 74), a pre-existing transmission line has already compromised the viewshed.

HR-9

Within Visual APE?: No	NRHP Status: Ineligible	Adverse Effect: No
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Located at 2738 Ellington Airline Drive, south of the proposed transmission line, this one-story wood frame, hall-parlor was constructed in ca. 1910 (see Figure 1, Sheet 1). The structure is clad



Figure 71. HR-8, view south-southwest.



Figure 72. HR-8, view northwest.

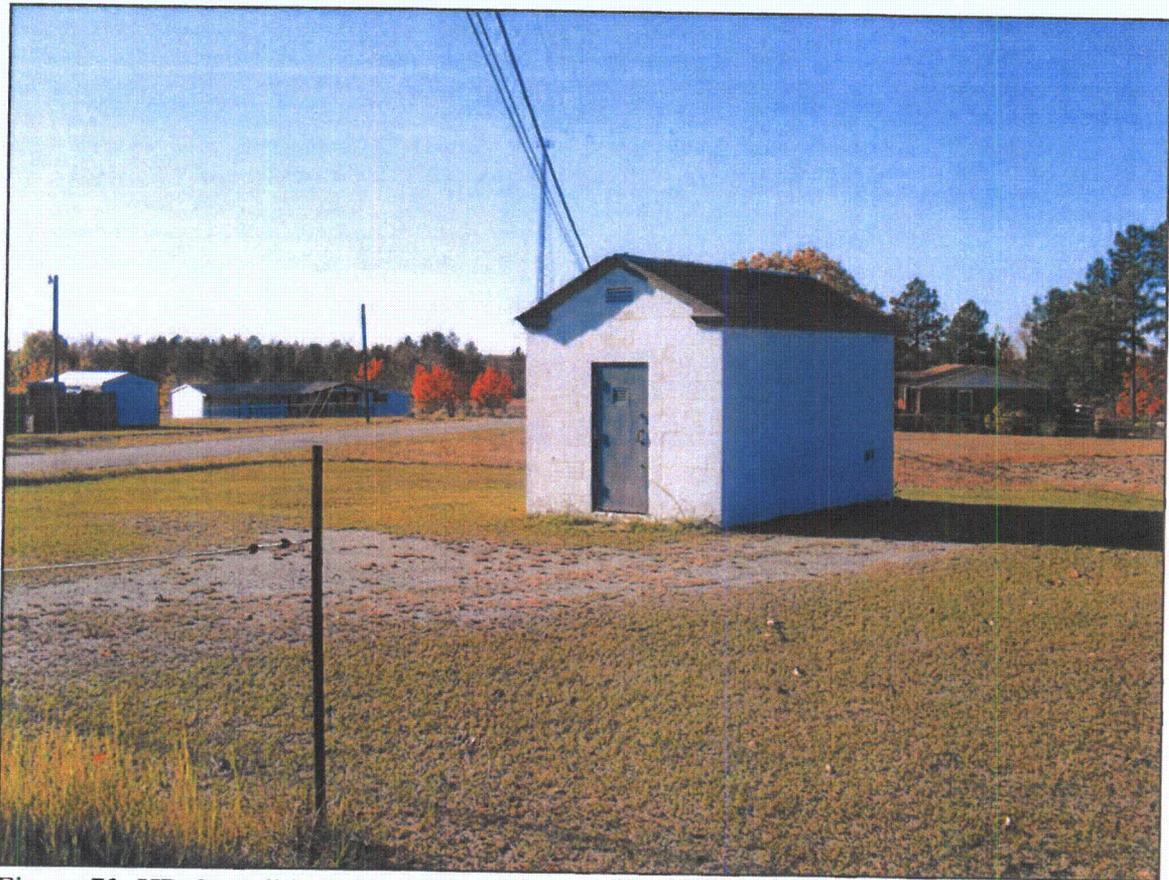


Figure 73. HR-8, well house, view southwest.

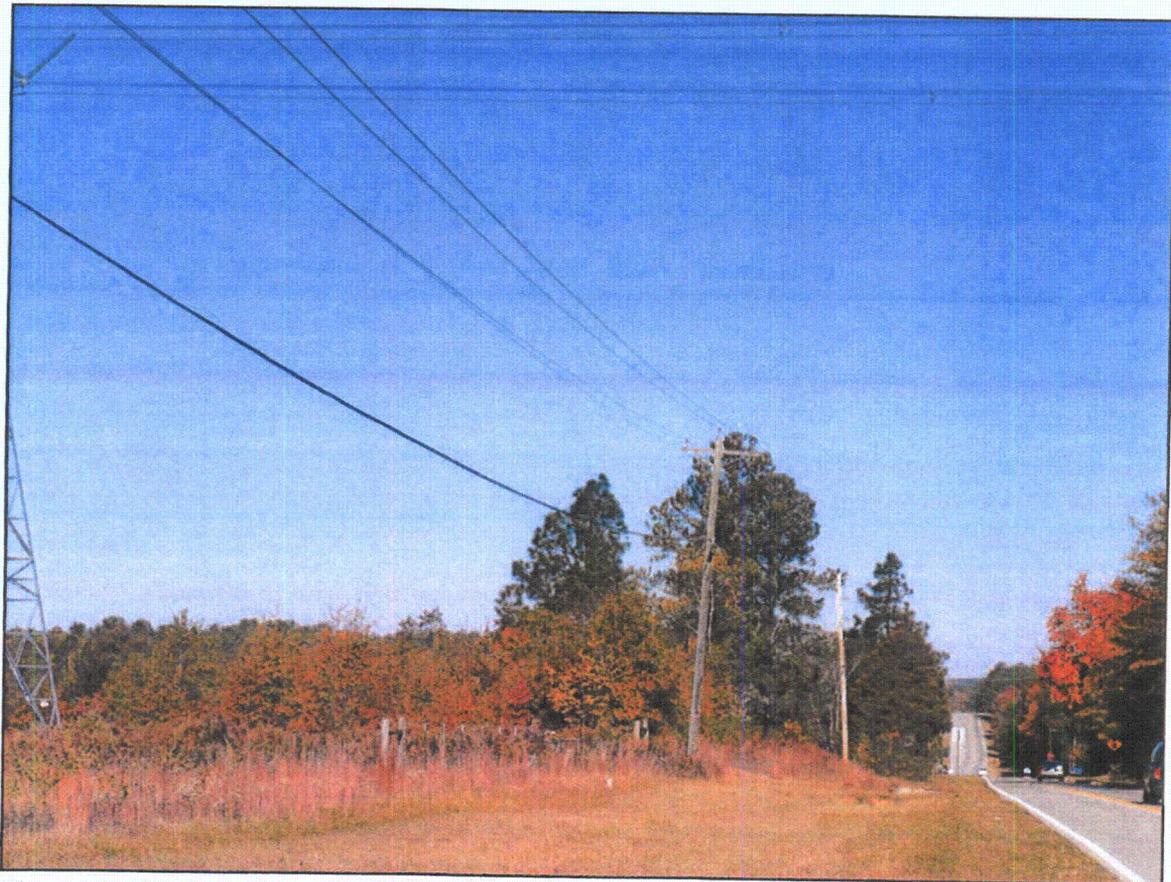


Figure 74. HR-8, view to project area, north.

in vinyl siding and rests on a concrete block foundation (Figure 75). It has a replacement standing seam metal roof with an external brick chimney on the south elevation. Fenestration includes nine-over-nine double-hung wood sash windows. The main access is through a single leaf replacement door off a nearly full-width porch. The porch features a shed roof supported by columns on a concrete base accessed via brick steps. Among the additions to the structure is a ca. 1930 rear wing featuring materials consistent with the main block (Figure 76). Other additions include a ca. 1940 addition within the ell with a shed-style roof, and one-over-one aluminum windows. Secondary entry is gained through a single leaf replacement door on the south end of the ell addition. Attached to the east elevation (gable end) of the ell is an addition with a shed roof.

NRHP Assessment: The additions and the subsequent introduction of new materials have impacted HR-9's integrity of materials, workmanship, feeling, and design. The resource retains integrity of location, association, and setting. The building exhibits unremarkable design and use of materials. In accordance with 36 CFR 60.4, the HR-9 dwelling is recommended ineligible for the NRHP. This resource is not associated with an event or series of events, or a person(s) significant on the national, state, or local level and is not eligible for the NRHP under Criteria A or B. The resource does not embody the distinctive characteristics of a style, period, or method of construction, nor are they the work of an architect or master builder and are not eligible for the NRHP under Criterion C. The proposed transmission line is not visible from HR-9.

HR-11

Within Visual APE?: Yes	NRHP Status: Ineligible	Adverse Effect: No
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This isolated historic barn is located on the east side of Kelly Road in McDuffie County, east of the proposed transmission line (see Figure 1, Sheet 2). Constructed in ca. 1940, this one-story wood frame, three-bay front gable barn is clad in horizontal boards and covered with a standing seam metal roof (Figures 77 and 78). It is unclear if the dwelling associated with the property has been demolished, or if one of the surrounding properties, which have since been subdivided, once included the barn. The 1950 USGS Bowdens Pond 7.5-minute quadrangle shows three structures near this location, while the area surrounding the barn is currently composed of agricultural fields.

NRHP Assessment: While the resource retains integrity of location, it no longer retains its historic context due to the loss of any associated historic dwelling or other outbuildings. It does not embody a significant building type or display notable design features or materials. In addition to this, the barn has been neglected, and is no longer in good repair. For these reasons, TRC recommends HR-11 ineligible for the NRHP. Although the viewshed of the structure will be affected by the proposed undertaking, based on the NRHP eligibility assessment, no further work is recommended for this resource (Figure 79).

HR-12 Phillips Cemetery

Within Visual APE?: Yes	NRHP Status: Ineligible	Adverse Effect: No
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The Phillips Cemetery is located off Kelly Road in McDuffie County, west of the proposed transmission line (see Figure 1, Sheet 2). Containing approximately 20 individuals,



Figure 75. HR-9, view southwest.



Figure 76. HR-9, view north-northeast.



Figure 77. HR-11, view northeast.



Figure 78. HR-11, view northwest.



Figure 79. HR-11, view to project area, southwest.

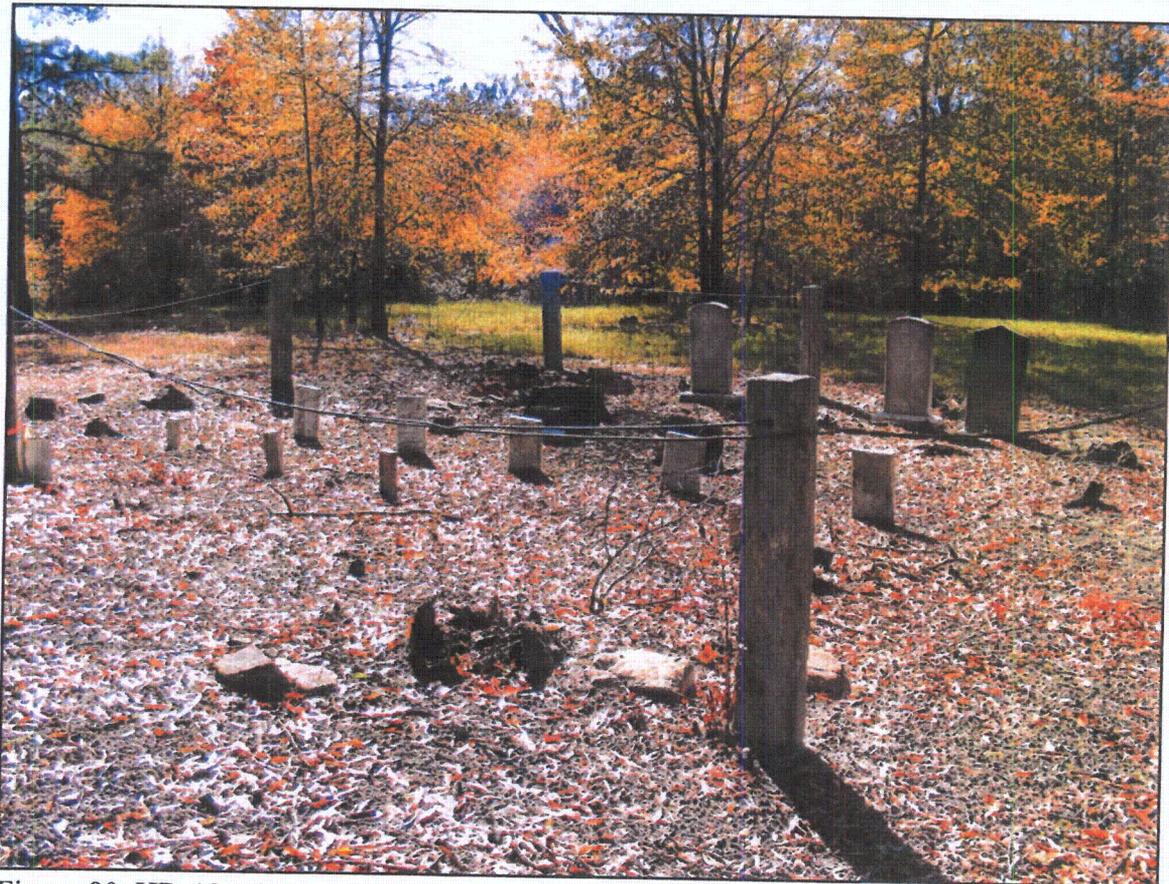


Figure 80. HR-12, view southwest.

predominantly from the Phillips family, the interments date from 1899–1920. This recently rehabilitated cemetery has some new head and foot stones of marble set in concrete bases, as well as some historic marble and fieldstone markers (Figure 80). Enclosed by wire rope threaded through cut-stone posts, the cemetery is situated in a cleared area of trees, with no other landscaping.

NRHP Assessment: The furnishings of the Phillips Cemetery, some of which are not original, are unremarkable, as is the landscape setting. Historical research does not associate the cemetery with an event or series of events, or with a person(s) significant on the national, state, or local level. Thus, the resource does not satisfy Criteria Consideration A. In accordance with 36 CFR 60.4, HR-12 is considered ineligible for the NRHP. The resource is not associated with an event or series of events, or with a person(s) significant on the national, state, or local level; thus, it is not eligible for the NRHP under Criteria A and B. The resource does not embody the distinctive characteristics of a style, period, or method of construction, nor is it the work of an architect or master builder and is not eligible for the NRHP under Criterion C. For these reasons, no further work is recommended for this resource. Although the viewshed of the cemetery will be affected by the proposed undertaking (Figure 81), based on the NRHP eligibility assessment, no further work is recommended for this resource.

HR-13

Within Visual APE?: Yes	NRHP Status: Ineligible	Adverse Effect: No
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Located west of the proposed transmission line at 299 Hillman Road is this one-story cross gable bungalow (see Figure 1, Sheet 2). Constructed in 1935 (WCTA), the structure rests on brick piers with concrete block infill and is clad in weatherboard with a standing seam metal roof. There is an external brick chimney on the south elevation, and a ca. 1940 external brick chimney on the north elevation. Fenestration consists of six-over-six double hung wood sash windows with decorative shutters. The main entry is through a single leaf door within the partial width front gable porch. The porch roof is supported by wood posts on brick piers resting on a concrete base with brick steps and foundation. A secondary entrance is on the south elevation through a single leaf door covered by a gable roof supported by posts which rest on a concrete stoop. A ca. 1940 shed roof addition has been added to the east elevation; materials are consistent with the main block, except where a former door opening has been filled with plywood (Figures 82 and 83).

Shed: This ca. 1935 one-story front gable wood frame structure lies to the east of the main dwelling. Resting on stone piers, it is covered in vertical board and has a standing seam metal roof. Entry is gained through a hinged wood plank door on the west gable end and an open shed addition is present on the west gable end (Figure 84).

Garage: This ca. 1940 one-story, one bay front gable wood frame garage is covered in weatherboard with a standing seam metal roof. An additional bay with a shed-style roof and vertical board has been added to the south elevation (Figure 85).

NRHP Assessment: HR-13 has retained its integrity of location, materials, workmanship, feeling, and association. However, modifications including the secondary entrance and the rear addition have compromised the dwelling's integrity of design, in spite of the fact that the changes were made relatively early in the resource's history. HR-13 and its outbuildings are common examples



Figure 81. HR-12, view to project area, southeast.

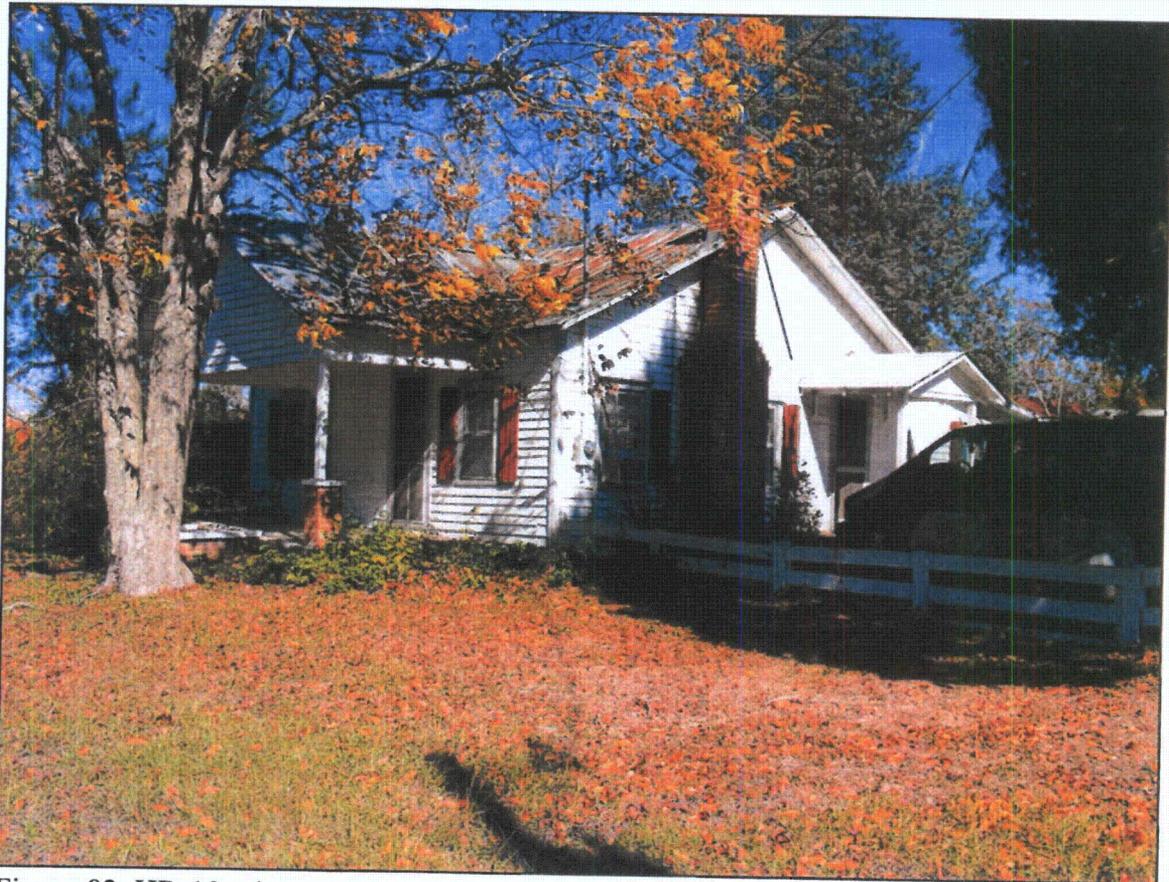


Figure 82. HR-13, view northeast.



Figure 83. HR-13, view northwest.

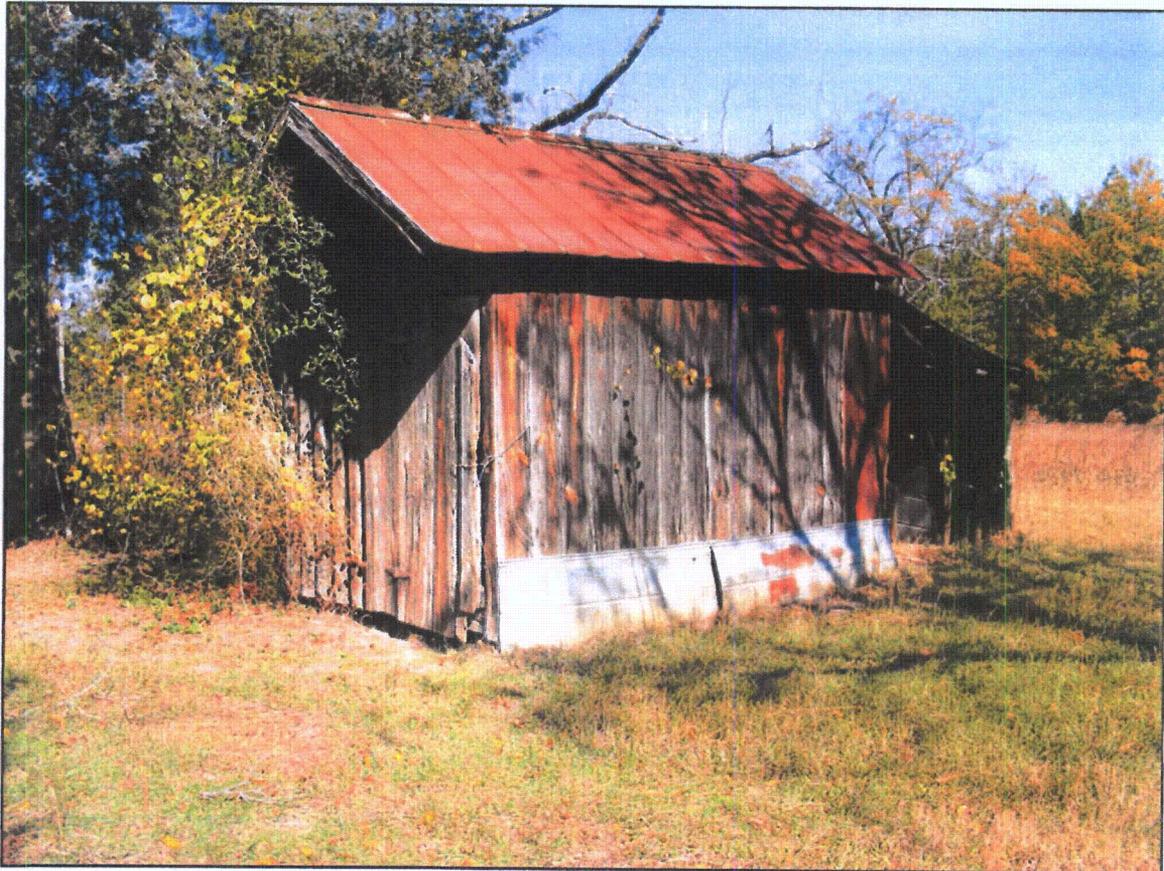


Figure 84. HR-13, shed, view northeast.



Figure 85. HR-13, garage, view northeast.



Figure 86. HR-13, view to project area, southeast.

of their type and do not include the range of historic agricultural outbuildings that might make the property a good example of a period farmstead. For these reasons, HR-13 is recommended ineligible for consideration for NRHP Criterion C. Background research undertaken by TRC has indicated that the property does not qualify for the NRHP under Criteria A or B. The research identified no known associations with significant historical events or persons. Although the proposed transmission line will be visible from this resource, an existing transmission line has already compromised the viewshed (Figure 86). Based on the NRHP assessment, TRC recommends no further consideration of this resource in advance of the proposed undertaking.

HR-14

Within Visual APE?: Yes	NRHP Status: Potentially Eligible	Adverse Effect: No
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HR-14 is located at 1317 Hillman Road, west of the proposed transmission line (see Figure 1, Sheet 3). Built in ca. 1900, this one-story, wood frame, front gable bungalow rests on brick piers with concrete block infill, and is covered in wavy edge asbestos. The roof is clad in standing seam metal and features exposed rafter tails. There are two external brick chimneys on the south elevation, likely dating from ca. 1930, and one internal brick and concrete chimney on the south slope, contemporary to the original construction. Fenestration on the structure consists of six-over-six double hung wood sash windows as well as some one-over-one double hung aluminum sash replacement windows. There are louvered attic vents in the gable ends. The primary entrance is through a single leaf door accessed through a partial width screened in porch with an aluminum hood covering the steps. A secondary entrance, on the east elevation is through a partially enclosed porch with six-over-six double hung aluminum sash windows. The north end of the porch has been enclosed and turned into living space. Attached to the living space portion of this porch is a shed addition whose roof extends to cover a patio which is supported by metal filigree posts. The addition has six-over-six double hung wood sash windows and a concrete block foundation (Figures 87 and 88). This resource includes seven outbuildings (Figure 89) as discussed below, and is associated with HR-15, the D. S. Hillman & Sons Store. The building and associated outbuildings are abandoned and have begun to fall into varying degrees of disrepair.

Barn 1: This ca. 1930 one-and-a-half story, wood frame, front gable barn is composed of flush board and has a standing seam metal roof. Entry is gained through an opening on the north gable end, where there is also a hayloft opening. An open shed bay is present on the west elevation. It has been abandoned and begun to fall into disrepair (Figure 90).

Outbuilding 1: Built in ca. 1940, this one-story wood frame structure is covered in flush board and has a corrugated metal roof. Entry is gained through hinged double doors, which are constructed of wood planks (Figure 91).

Outbuilding 2: Resting on stone piers, this ca. 1920 one-and-a-half story wood frame structure is covered in flush board. The front gable structure has a metal roof. An open bay with a shed roof is attached to the north elevation. Entry is gained through a hinged metal door in the west gable end, where there is also a hayloft opening (Figure 92).



Figure 87. HR-14, view northeast.



Figure 88. HR-14, view southwest.

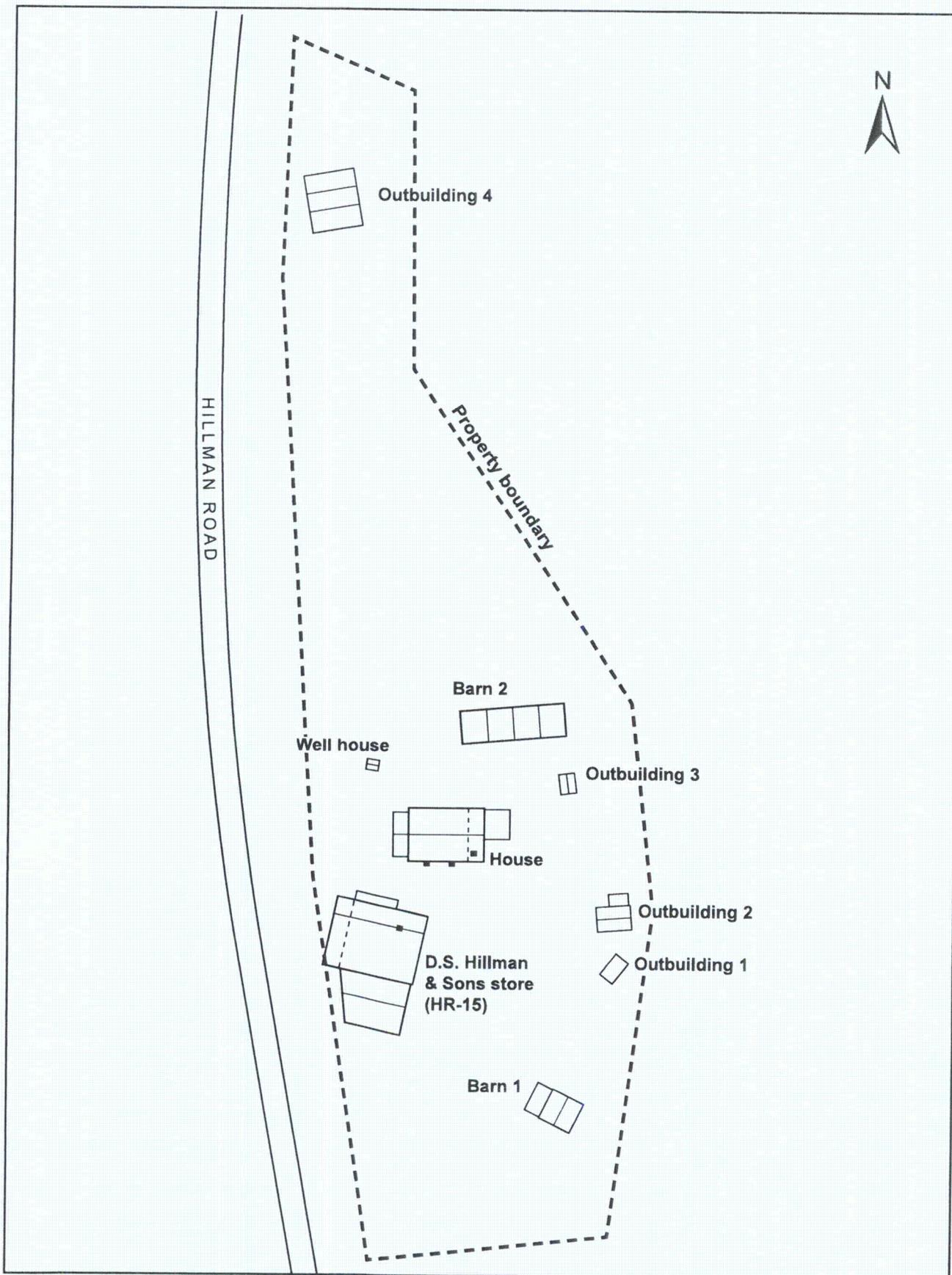


Figure 89. HR-14 site plan.

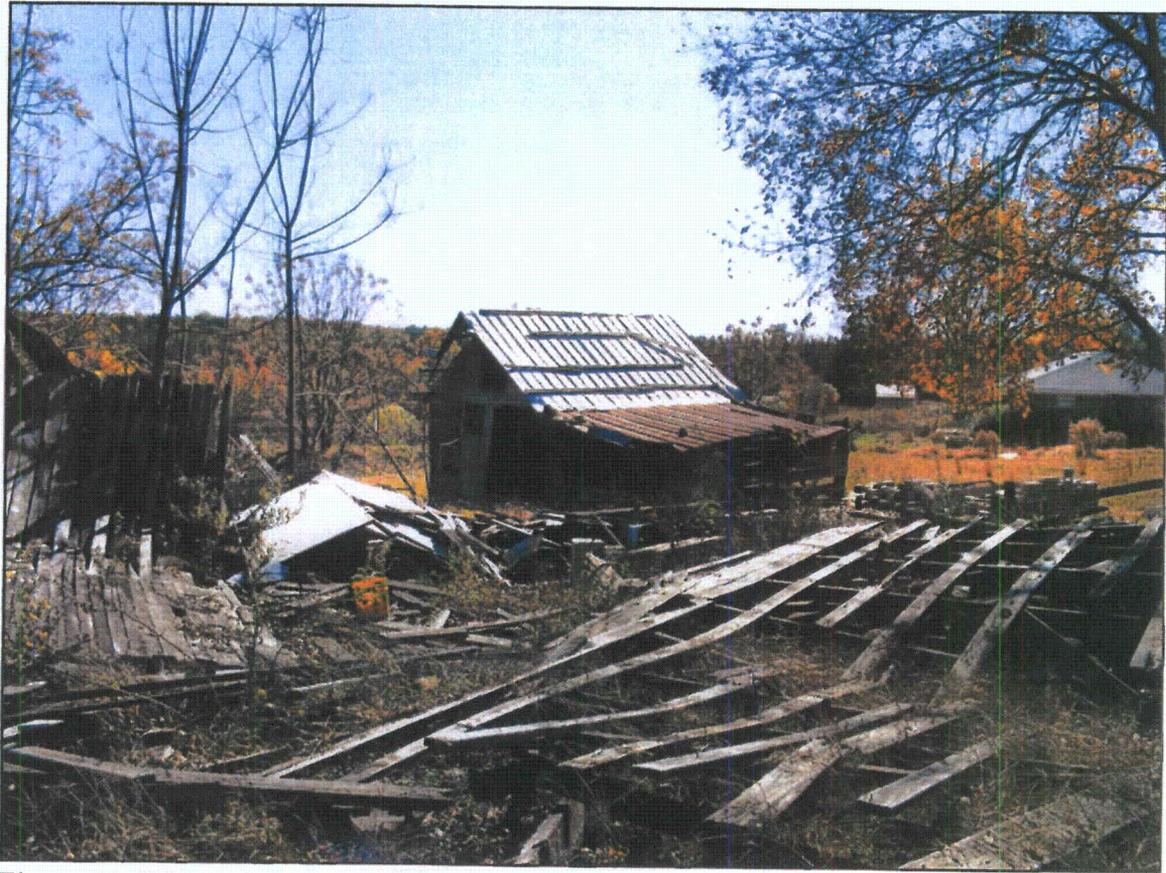


Figure 90. HR-14, barn 1, view south-southeast.



Figure 91. HR-14, outbuilding 1, view southeast.



Figure 92. HR-14, outbuilding 2, view southeast.

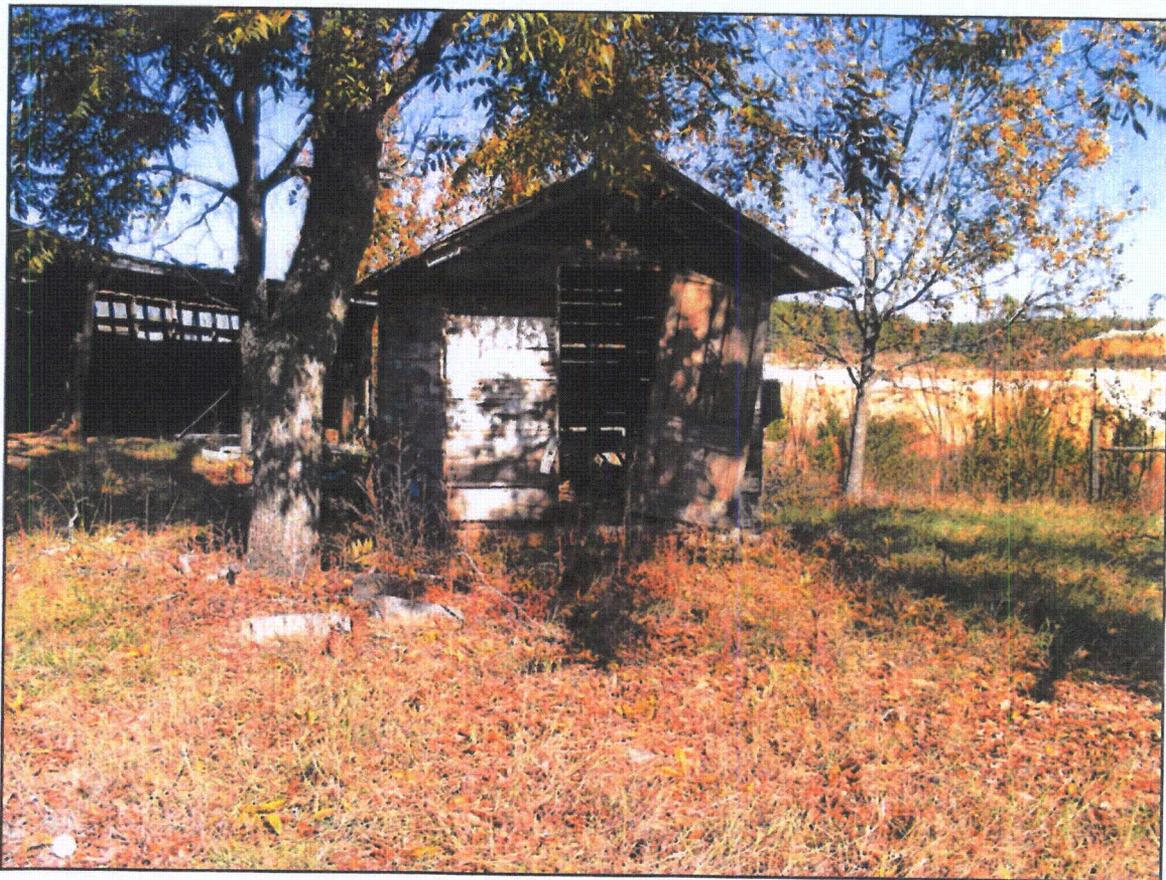


Figure 93. HR-14, outbuilding 3, view north-northeast.

Outbuilding 3: This small wood frame front gable structure rests on a concrete foundation. Constructed in ca. 1940, the walls are clad in flush board, and the roof is metal. Entry is gained through a hinged single leaf door on the south gable end (Figure 93).

Barn 2: Constructed in ca. 1950, this one-and-a-half story, five bay, wood frame barn is clad in flush board and metal. The front gable roof is standing seam metal. The west side of the central bay is enclosed with flush board on the top, and vertical metal on the lower portion. Entry is gained through a pair of hinged wood frame doors covered with metal. The remainder of the bays on the structure are open; including the shed additions on the east and west elevations (Figure 94).

Well House: Built in ca. 1960, this front gable brick well/pump house has a plywood door and a compositional asphalt roof. It likely replaces an earlier structure which would have stood at the same location (Figure 95).

Outbuilding 4: Located some distance from the other outbuildings, but still on the same property (see Figure 90), is this one-story, wood frame front gable structure built in ca. 1930. Perhaps once a residence, in later years the structure appears to have been used for storage and is now abandoned and falling into disrepair. Resting on brick piers, with some replacement concrete block piers, the structure is covered in BrickTex siding and has a standing seam metal roof with exposed rafter tails. There are two entrances on the façade, which were once accessed through a now removed porch. A shed-roof bay has been added to the south elevation (Figure 96).

NRHP Assessment: TRC recommends HR-14 and its outbuildings eligible for the NRHP. The resource has retained its integrity of location, design, materials, workmanship, feeling, and association. A few major modifications have been made to the dwelling over time, but the modifications were carried out historically (e.g., the rear shed addition, the newer chimneys, and enclosing the porches). HR-14 is eligible under NRHP Criterion C, as it retains most of its original features and represents a good example of its type. Furthermore, the dwelling is associated with resource HR-15, a potentially eligible rural hardware store, and is therefore eligible under Criterion A. Although the dwelling and outbuildings have largely retained their rural setting, the building is vacant and associated agricultural fields are no longer present, due to the presence of a quarry. For these reasons, the property no longer reflects an intact agricultural landscape (Messick et al. 2001). Background research undertaken by TRC has indicated that the property does not qualify for the NRHP under Criterion B. The research identified no known associations with significant persons. The NRHP-eligible boundary for HR-14 coincides with the legal tax parcel boundary for the property and contains the dwelling, outbuildings, and HR-15.

Assessment of Effects: Because the viewshed has been compromised by an open mine adjacent to the property, in TRC's opinion there will be no adverse effect to HR-14 created by the proposed project (Figure 97).

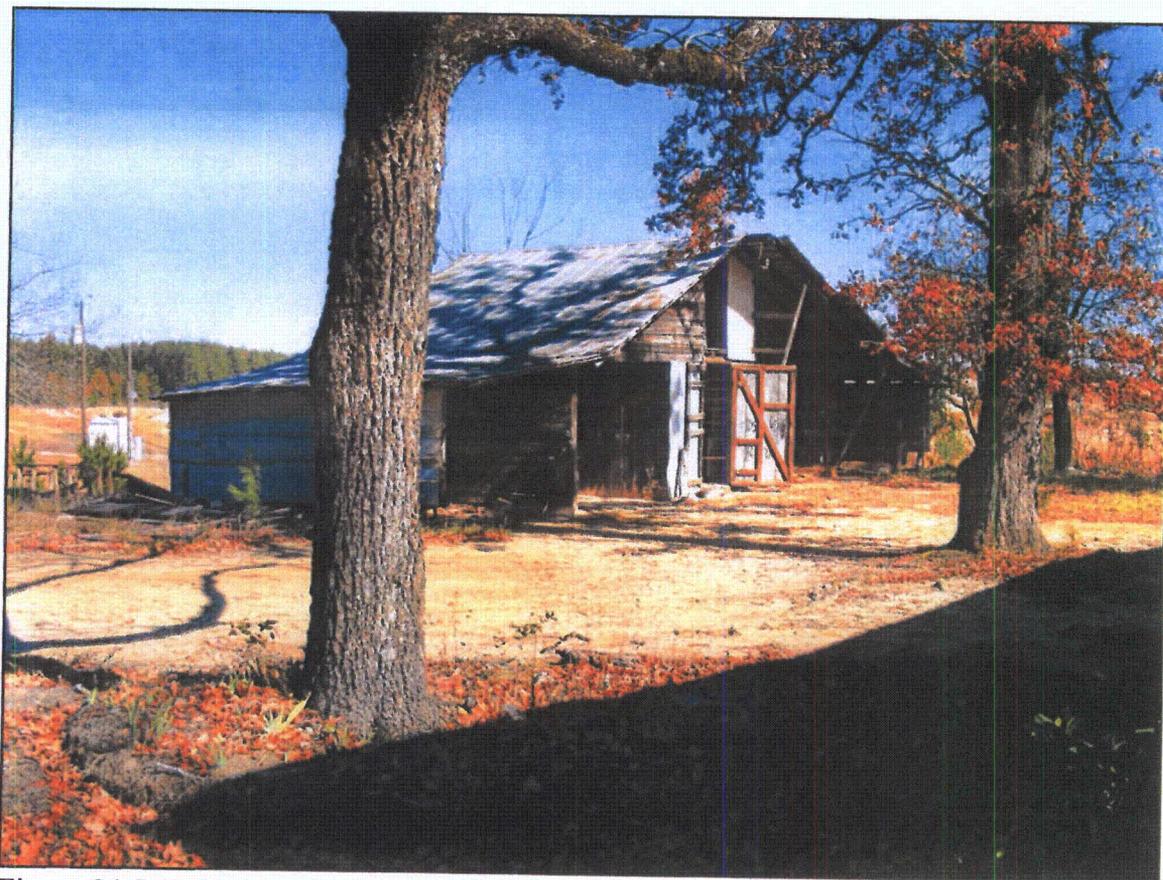


Figure 94. HR-14, barn 2, view northeast.

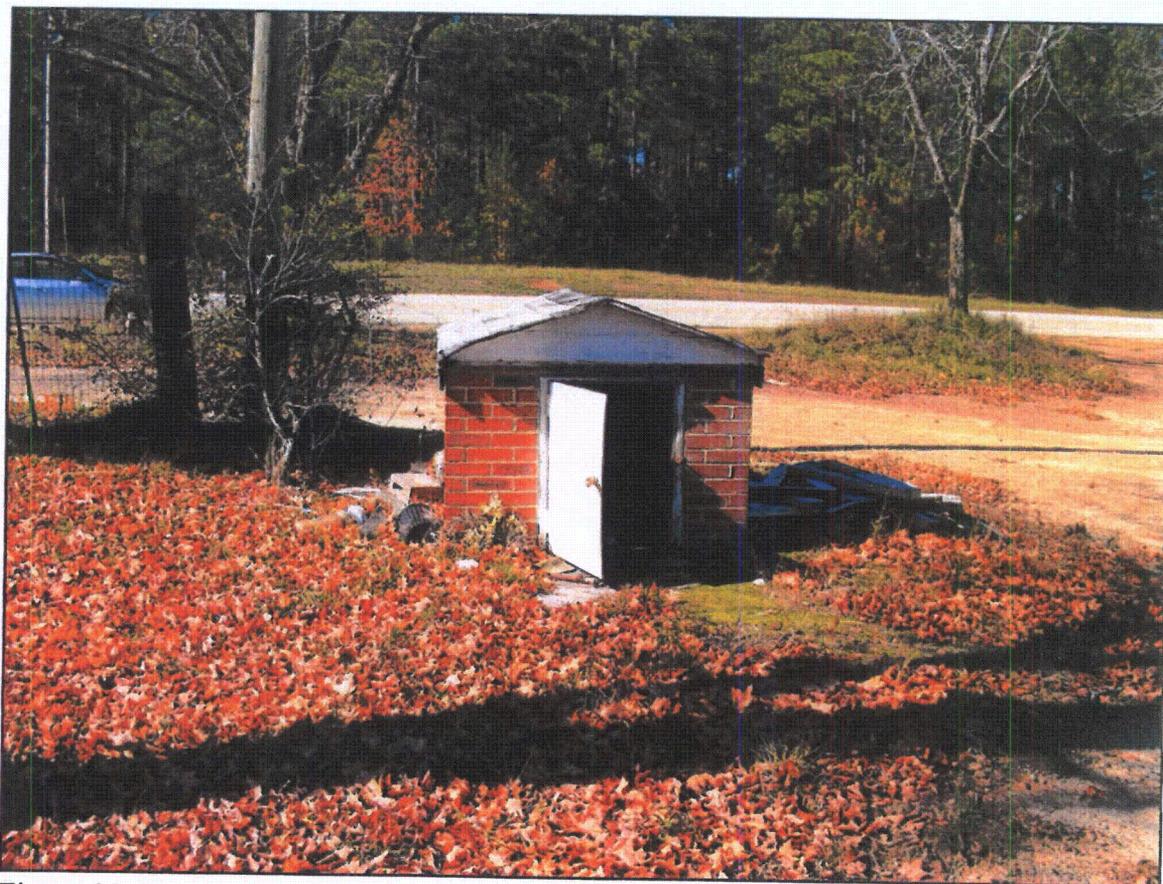


Figure 95. HR-14, well house, view west-northwest.



Figure 96. HR-14, outbuilding 4, view northeast.

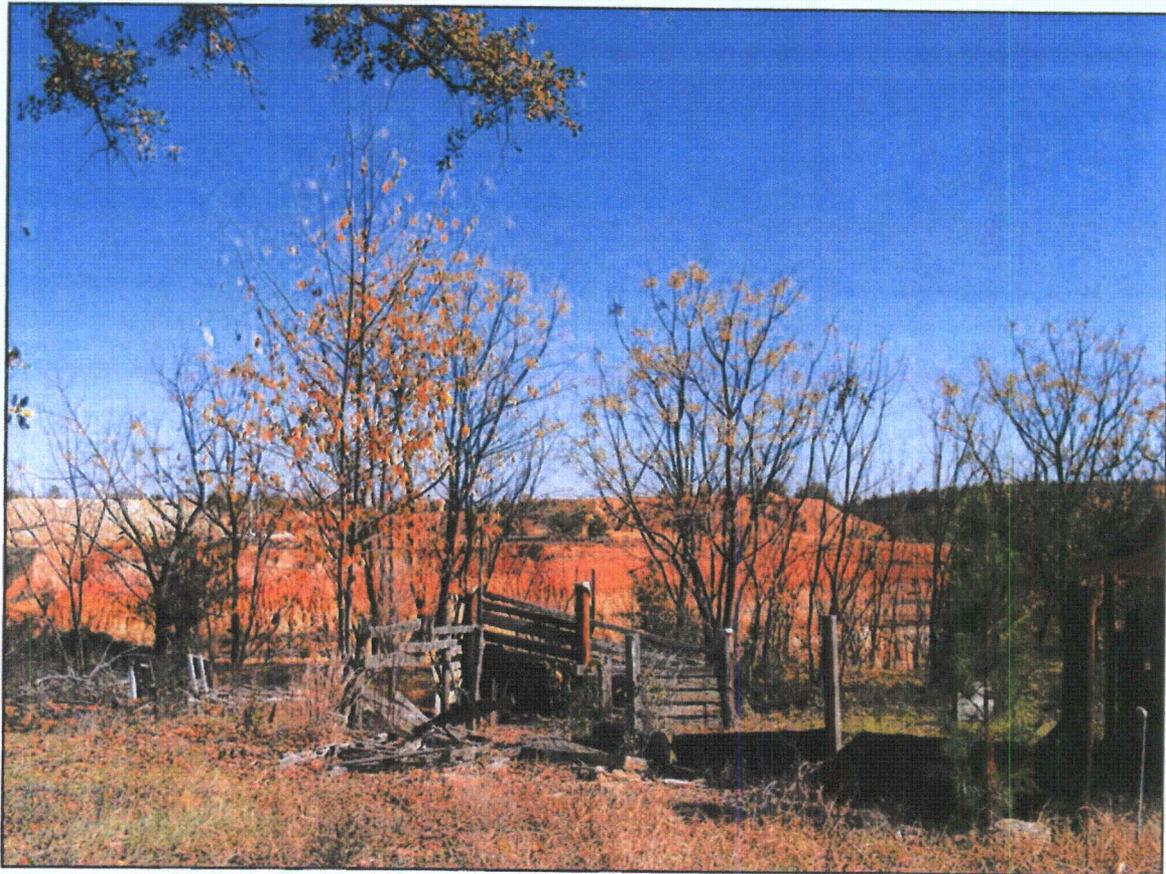


Figure 97. HR-14, view to project area, east.